

SEAL ISLAND ESTATES

TENTATIVE MAP CONTRA COSTA COUNTY, CA SUBDIVISION NO.

OWNER & SUBDIVIDER

DISCOVERY BUILDERS, INC.
43240 STONE VALLEY ROAD WEST, SUITE 2
ALAMO, CALIFORNIA 94507
CONTACT: KRIS KAMERZELL

PREPARER'S STATEMENT/DESIGN ENGINEER

THIS PLAN SET WAS PREPARED BY:
RRM DESIGN GROUP
3765 S. HIGUERA STREET
SAN LUIS OBISPO, CA 93401
(805) 543-1794

UNDER THE DIRECTION OF:
ROBERT CAMACHO, P.E. No. 76597

SHEET INDEX

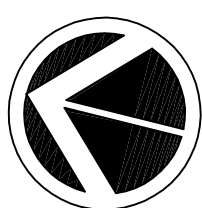
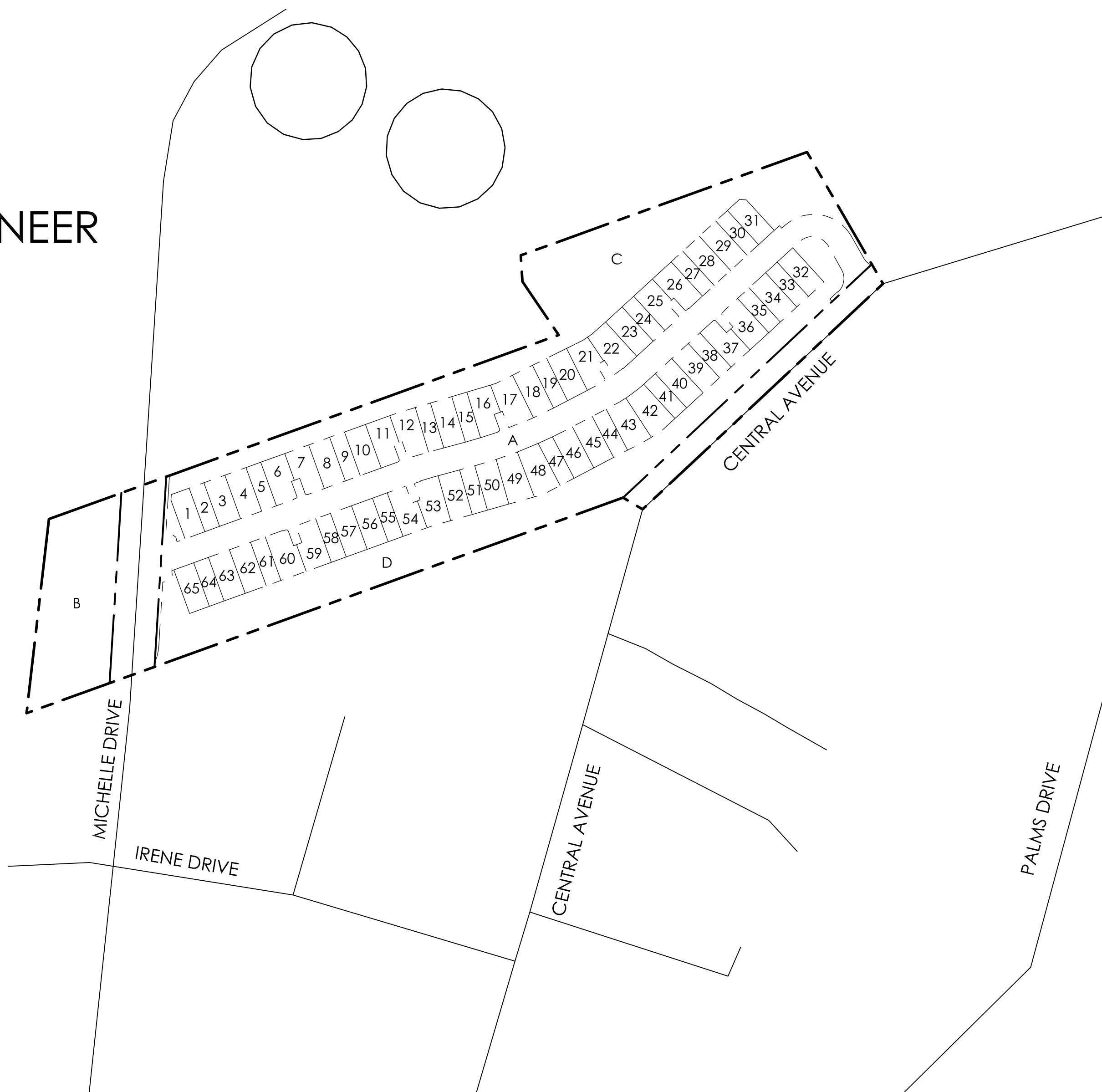
C1.....TITLE SHEET
C2.....EXISTING CONDITIONS MAP
C3.....VESTING TENTATIVE TRACT MAP
C4.....GRADING AND DRAINAGE
C5.....GRADING AND DRAINAGE
C6.....DETAILS
C7.....COMPOSITE UTILITIES
C8.....STORMWATER MANAGEMENT PLAN

PROPERTY INFORMATION

APN: 380-030-045
EXISTING GENERAL PLAN: H1 (HEAVY INDUSTRY)
EXISTING ZONING: H-1 (HEAVY INDUSTRIAL)
PROPOSED DENSITY: 12.6 DU/AC

UTILITIES

ELECTRIC..... PACIFIC GAS & ELECTRIC
TELEPHONE..... AT&T
CABLE..... CHARTER COMMUNICATIONS
GAS..... PACIFIC GAS & ELECTRIC
SEWER..... CENTRAL CONTRA COSTA SANITARY DISTRICT
WATER..... CONTRA COSTA WATER DISTRICT



VICINITY MAP

NOT TO SCALE

HORIZONTAL & VERTICAL CONTROL

BASIS OF BEARINGS

THE CALCULATED BEARING OF NORTH 46°14'08" WEST BETWEEN THE FOUND MONUMENTS C-90 AND C-102R LOCATED WITHIN INTERSTATE 680 RIGHT OF WAY, WAS CALCULATED FROM THE COORDINATE VALUES AS SHOWN ON THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION RECORD OF SURVEY MAP FILED IN BOOK 116 L.S.M. 21, IN THE OFFICE OF THE COUNTY RECORDER OF CONTRA COSTA COUNTY. SAID MAP VALUES ARE IN BOTH METRIC & FEET, AND ARE ON (CCS27), ZONE 3 VALUES.

SAID COORDINATE VALUES WERE SCALED TO GROUND BY THE SCALE FACTOR OF 1.00006140 PER SAID MAP. ALL DISTANCES SHOWN ARE GROUND UNLESS OTHERWISE NOTED.

TO CONVERT TO GRID HOLD POINT C-90 AND MULTIPLY BY 0.99993860

BENCHMARK

CCCCO BM #3550 TOP OF 'X' IN PHOENIX ON TOP OF DI, NORTH SIDE OF ACTRIZ AVE 80' WEST OF ARTHUR RD EL=23.35 (NGVD 29)

TOPOGRAPHY

TOPOGRAPHY AND MAPPING UNDER THE DIRECTION OF
APEX CIVIL ENGINEERING AND LAND SURVEYING
817 ARNOLD DRIVE, STE. 80
MARTINEZ, CA 94553

GENERAL INFORMATION/LAND USE

THIS SITE IS CURRENTLY UNDEVELOPED AND IS UNDERUTILIZED. SEAL ISLAND ESTATES PROPOSES 65 RESIDENTIAL LOTS IN AN ATTACHED, TOWNHOME-STYLE DEVELOPMENT. THE AVERAGE LOT SIZE IS PROPOSED TO BE 1,270 SF WITH A MINIMUM LOT SIZE OF 950 SF. WATER WILL BE SUPPLIED BY CONTRA COSTA WATER DISTRICT AND SEWER SERVICES WILL BE PROVIDED BY CENTRAL CONTRA COSTA SANITARY DISTRICT. THIS PROJECT FURTHERS THE COUNTY'S OBJECTIVES OF MEETING HOUSING DEMAND WHILE PROVIDING LOWER-INCOME DWELLING UNITS TO HELP FULFILL THE COUNTY'S RHNA OBLIGATIONS.

PROJECT STATISTICS

DESCRIPTION	LOT AREA (# OF LOTS)
RESIDENTIAL LOTS (LOTS 1-65)	1.89 ac (65)
OPEN SPACE LOTS (LOTS A-D)	2.78 ac (4)
TOTAL AREA	±4.67 ac (69)
TOTAL AREA DOES NOT INCLUDE MICHELE DRIVE RIGHT OF WAY AND PORTION OF CENTRAL AVENUE AREA RIGHT OF WAY	
TOTAL PARCEL AREA	±5.15 ac

TITLE SHEET

SEAL ISLAND ESTATES

C1

	TRACT BOUNDARY LINE
	PROPOSED LOT LINE
	RIGHT OF WAY LINE
	ADJACENT BOUNDARY LINE
	EASEMENT LINE
PUE	PUBLIC UTILITY EASEMENT
PSE	PRIVATE SEWER EASEMENT
STE	STREET TREE EASEMENT
PPAE	PUBLIC PEDESTRIAN ACCESS EASEMENT
	PROPOSED PRIVATE PEDESTRIAN ACCESS EASEMENT

PRIVATE VEHICULAR ACCESS, PRIVATE PEDESTRIAN ACCESS, PRIVATE PARKING, PRIVATE DRAINAGE OVER ALL OF LOTS A, B, C, AND D FOR THE BENEFIT OF THE PRESENT AND FUTURE OWNERS OF LOTS 1 THROUGH 65.

PUBLIC WATER, PUBLIC SEWER, PUBLIC UTILITIES, AND PUBLIC GAS EASEMENTS OVER ALL OF LOTS 6, 7, 11, 12, 16, 17, 21, 22, 25, 26, 36, 37, 42, 43, 48, 49, 53, 54, 59, 60, A, AND C.

PRIVATE STORM DRAIN AND PRIVATE DRAINAGE EASEMENTS OVER LOTS 1 THROUGH 65.

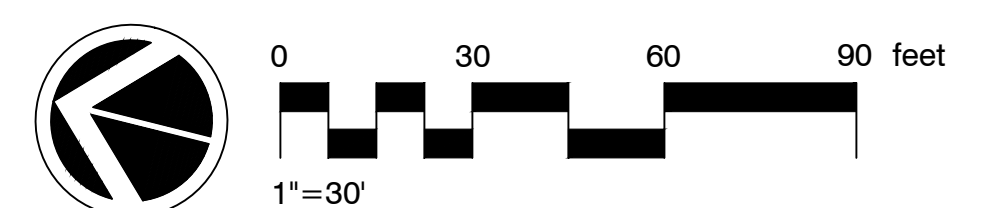
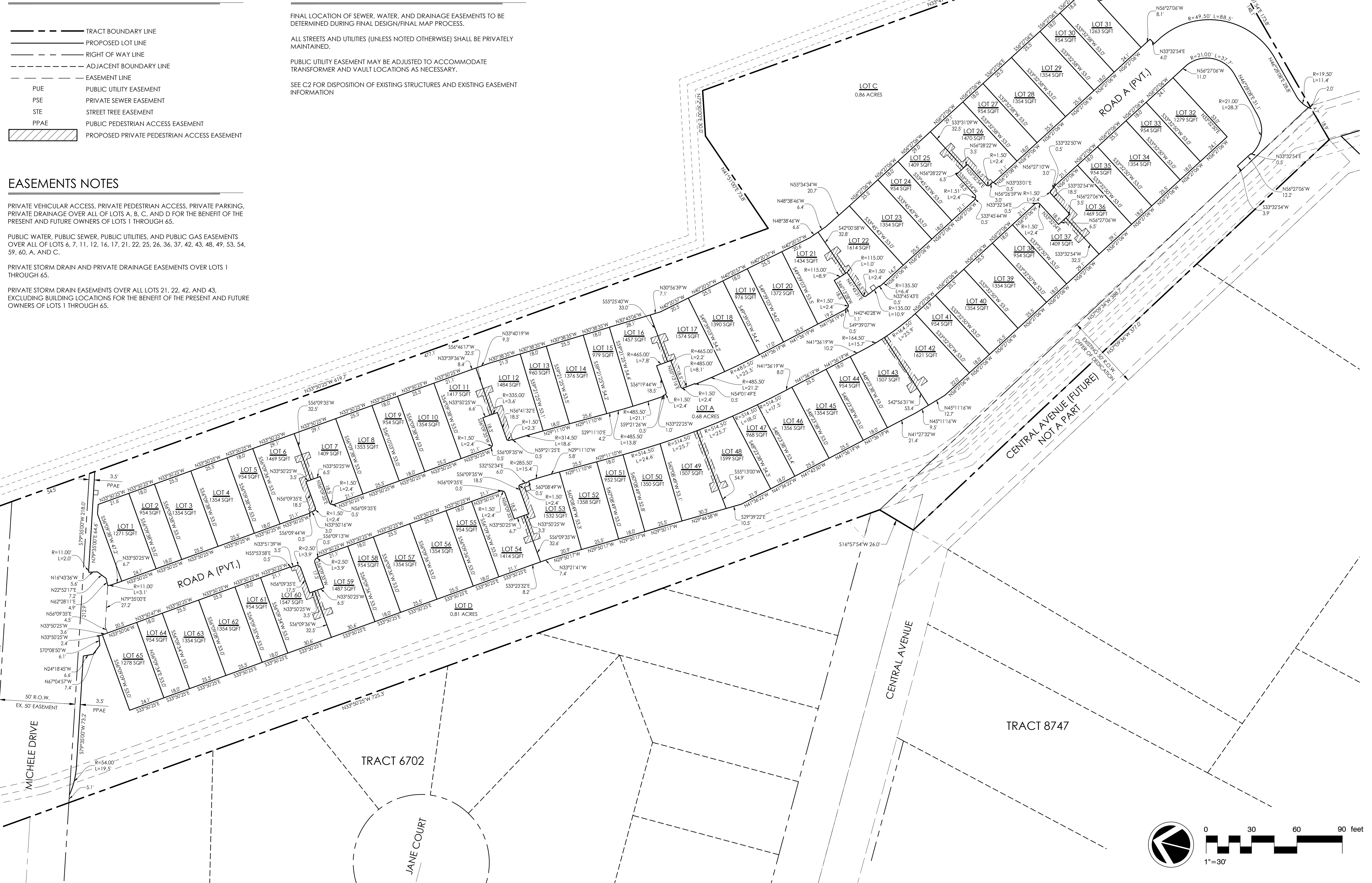
PRIVATE STORM DRAIN EASEMENTS OVER ALL LOTS 21, 22, 42, AND 43, EXCLUDING BUILDING LOCATIONS FOR THE BENEFIT OF THE PRESENT AND FUTURE OWNERS OF LOTS 1 THROUGH 65.

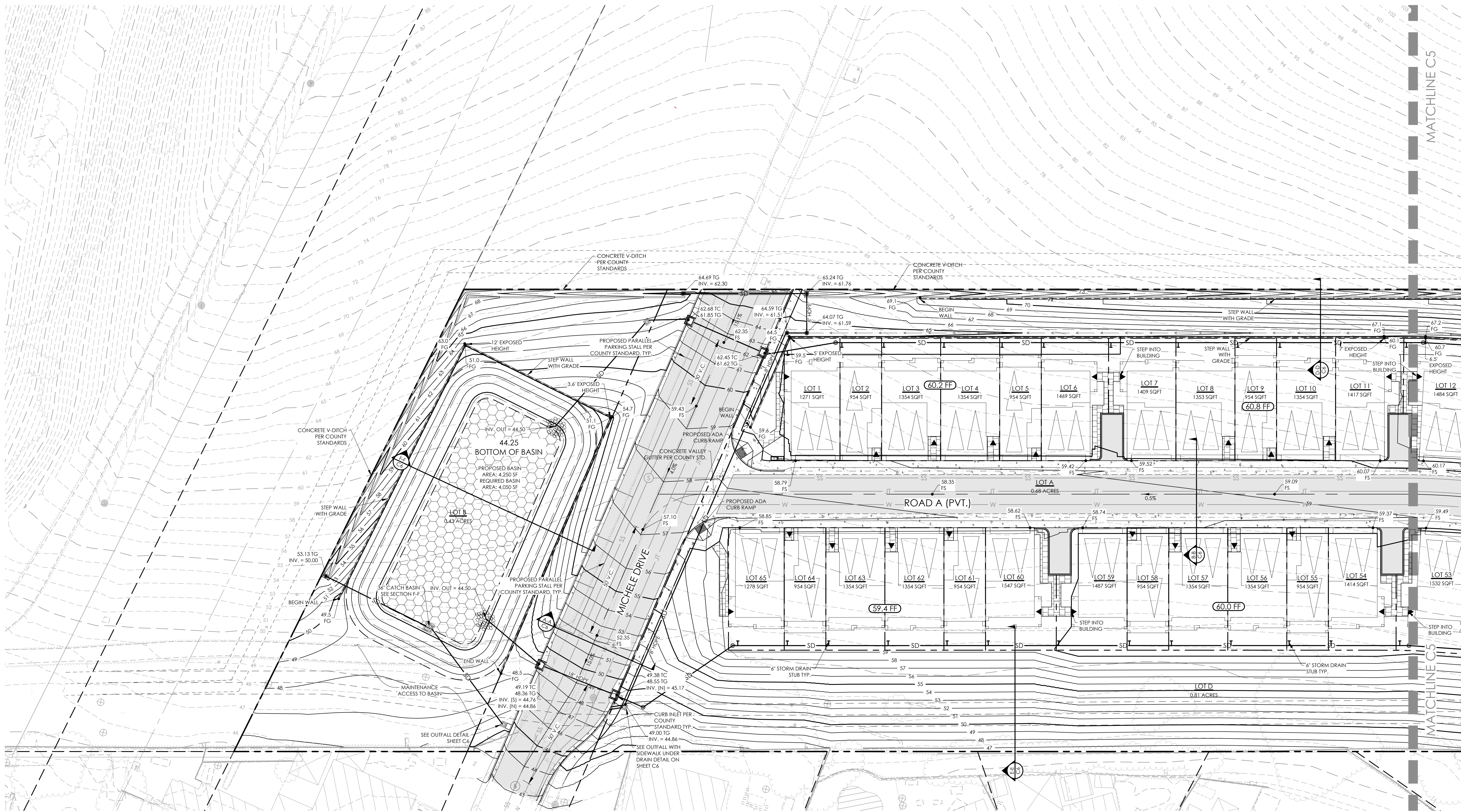
FINAL LOCATION OF SEWER, WATER, AND DRAINAGE EASEMENTS TO BE
 DETERMINED DURING FINAL DESIGN/FINAL MAP PROCESS.

ALL STREETS AND UTILITIES (UNLESS NOTED OTHERWISE) SHALL BE PRIVATELY
 MAINTAINED.

PUBLIC UTILITY EASEMENT MAY BE ADJUSTED TO ACCOMMODATE
 TRANSFORMER AND VAULT LOCATIONS AS NECESSARY.

SEE C2 FOR DISPOSITION OF EXISTING STRUCTURES AND EXISTING EASEMENT
 INFORMATION





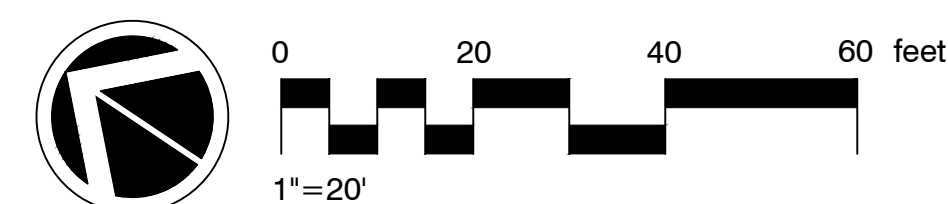
LEGEND

- STORM WATER BASIN
- AC PAVING
- REINFORCED CONCRETE
- DECORATIVE PAVERS

- SD PROPOSED PRIVATE 18" HDPE STORM DRAIN
- PROPOSED WALL (TOW = TOP OF WALL)
- AREA DRAIN
- TRAFFIC RATED TRENCH DRAIN
- MAILBOX KIOSK

GRADING NOTES

- DRIVEWAYS SHALL HAVE A MINIMUM SLOPE OF 1% AND A MAXIMUM SLOPE OF 10%.
- THERE SHALL BE A 0.5" LIP AT THE FRONT OF THE GARAGE.
- GARAGES SHALL HAVE A MINIMUM 1% SLOPE AND A MAXIMUM OF 2% SLOPE.
- STEP GARAGES BETWEEN UNITS WITH GRADE.





LEGEND

- | | | | |
|--|---------------------|--|---------------------------------------|
| | STORM WATER BASIN | | PROPOSED PRIVATE 18" HDPE STORM DRAIN |
| | AC PAVING | | PROPOSED WALL (TOW = TOP OF WALL) |
| | REINFORCED CONCRETE | | AREA DRAIN |
| | DECORATIVE PAVERS | | TRAFFIC RATED TRENCH DRAIN |
| | | | MAILBOX KIOSK |

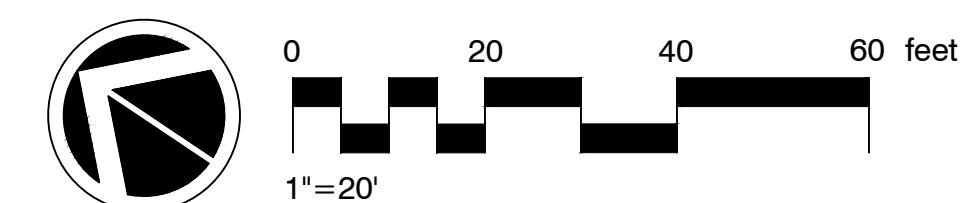
GRADING NOTES

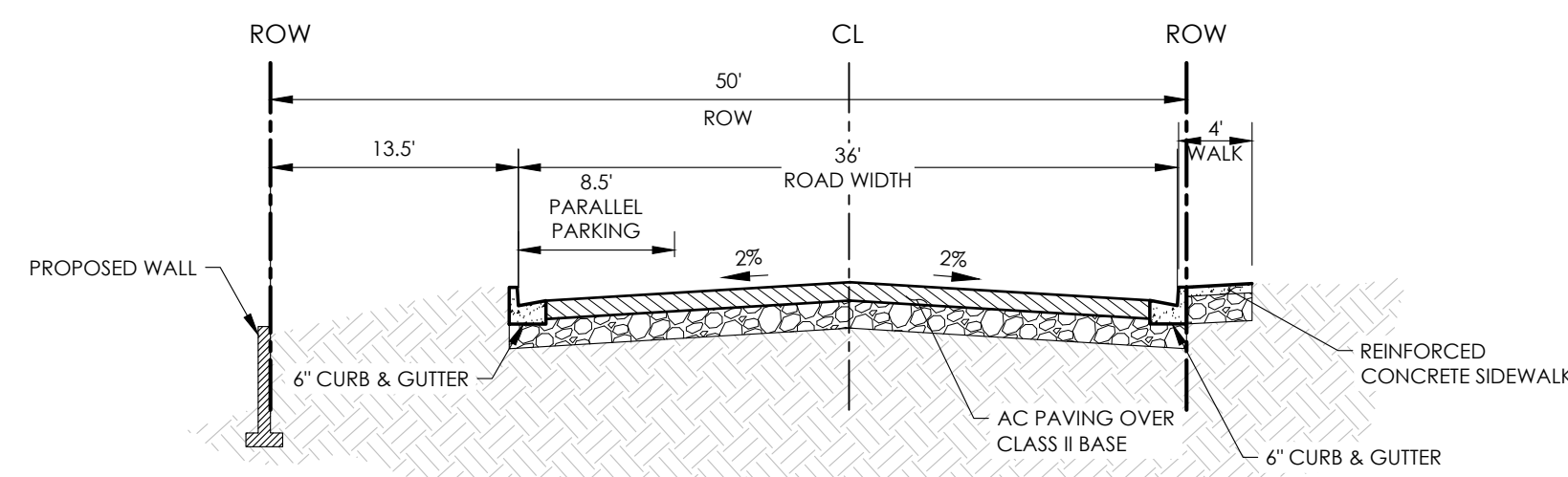
1. DRIVEWAYS SHALL HAVE A MINIMUM SLOPE OF 1% AND A MAXIMUM SLOPE OF 10%.
2. THERE SHALL BE A 0.5" LIP AT THE FRONT OF THE GARAGE.
3. GARAGES SHALL HAVE A MINIMUM 1% SLOPE AND A MAXIMUM OF 2% SLOPE.
4. STEP GARAGES BETWEEN UNITS WITH GRADE.

APPROXIMATE EARTHWORK QUANTITIES

AREA OF DISTURBANCE: 4.91 ACRES
CUT: 25,100 CY
FILL: 13,100 CY
NET: 12,000 CY <CUT>

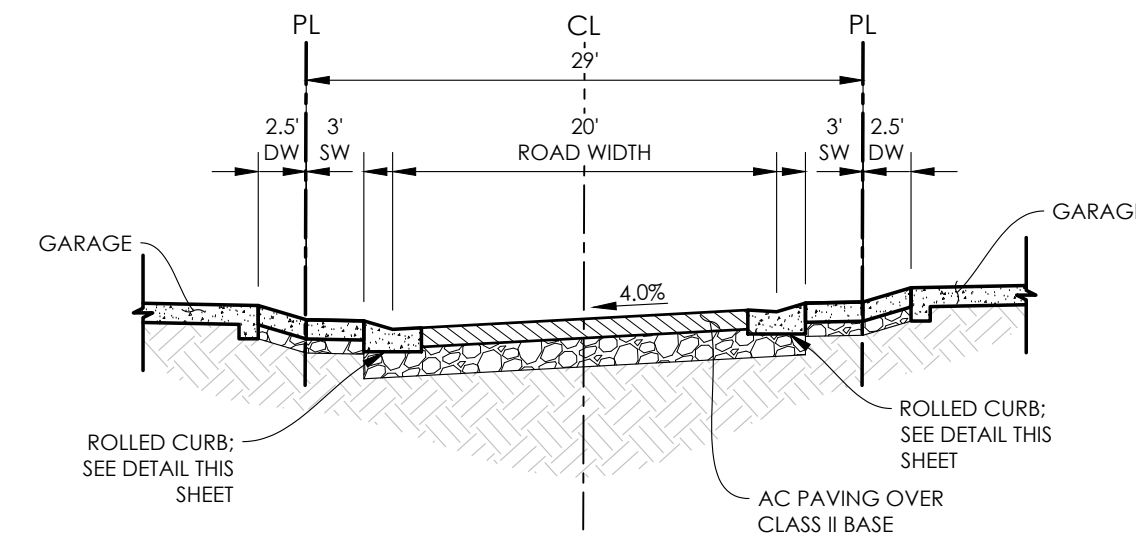
THE APPROXIMATE RAW EARTHWORK QUANTITIES SHOWN HEREON REPRESENT THE ESTIMATED VOLUMETRIC DIFFERENCE CALCULATED BETWEEN THE PROPOSED SUBGRADE AND EXISTING GRADE SURFACE, AND ARE SUBJECT TO CHANGE. THESE ESTIMATES DO NOT INCLUDE CONSIDERATIONS FOR LOSSES OR BULKING DUE TO: SOIL AMENDMENTS, STABILIZATION, CONSTRUCTION TECHNIQUE, FOOTING & TRENCHING SPOILS, ETC. THESE CONSIDERATIONS, IN ADDITION TO ACTUAL FIELD CONDITIONS AND THE FINAL RECOMMENDATIONS OF THE GEOTECHNICAL ENGINEER, MAY SIGNIFICANTLY EFFECT THE FINAL IMPORT/EXPORT QUANTITIES. APPROXIMATE QUANTITIES SHOWN ON THESE PLANS ARE FOR PERMITTING PURPOSES ONLY. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CALCULATE ACTUAL QUANTITIES FOR THE PURPOSE OF CONSTRUCTION AND COST ESTIMATES. CONTRACTOR IS ALSO RESPONSIBLE FOR ADJUSTMENTS TO SLOPE HINGE POINTS IN ORDER TO PROVIDE GRADED PAD AREA ADJACENT TO PATHS, WALKWAYS, AND ROADS FOR UTILITY BOXES, TRANSFORMERS, AND ABOVE GROUND UTILITY INFRASTRUCTURE.





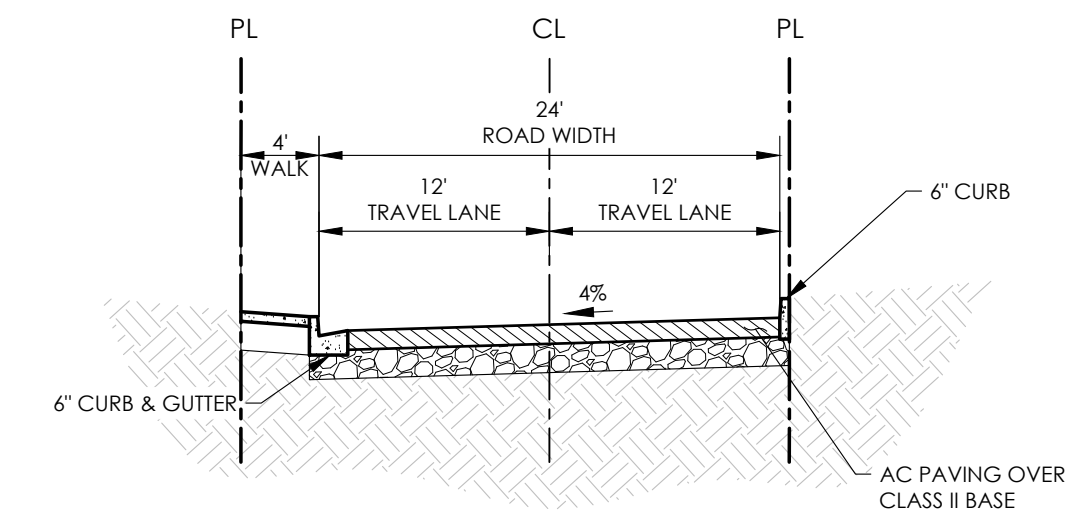
SECTION (A-A) - MICHELE DRIVE (PUBLIC)

NTS



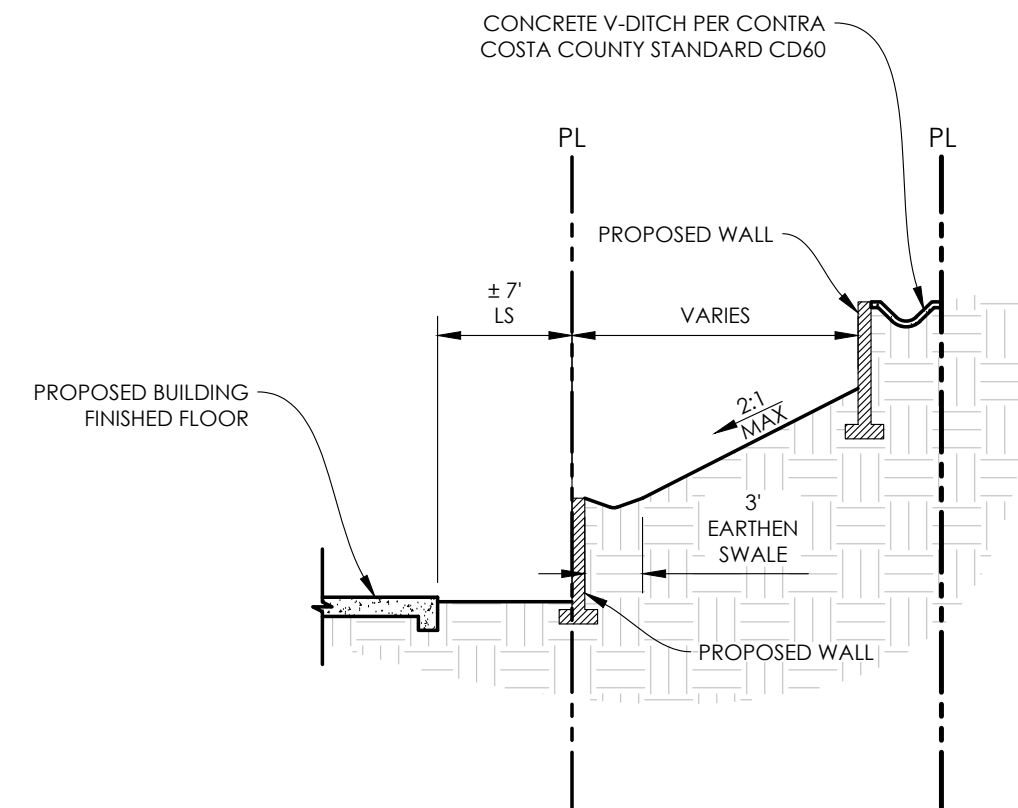
SECTION (B-B) - ROAD A (PRIVATE)

NTS



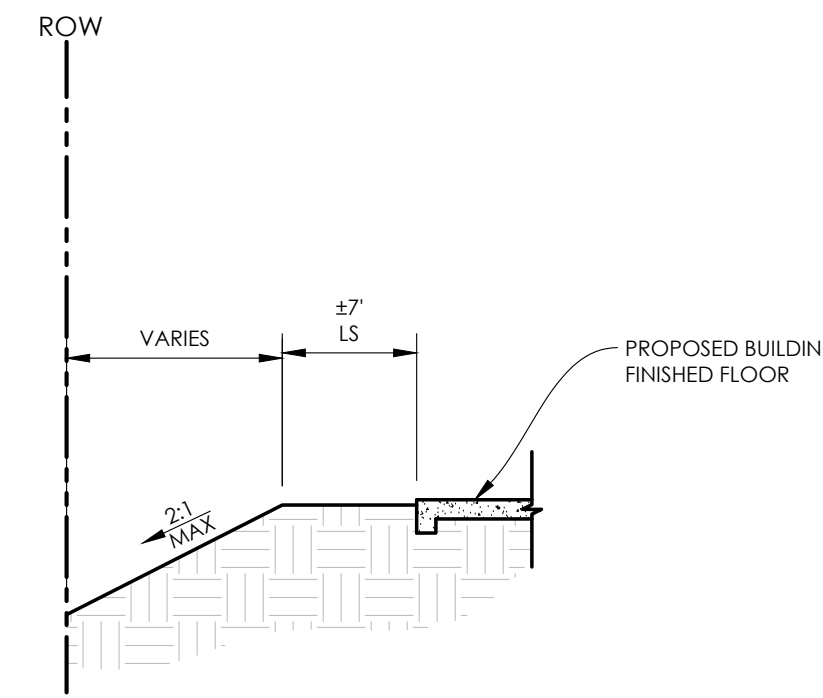
SECTION (C-C) - ROAD A (PRIVATE)

NTS



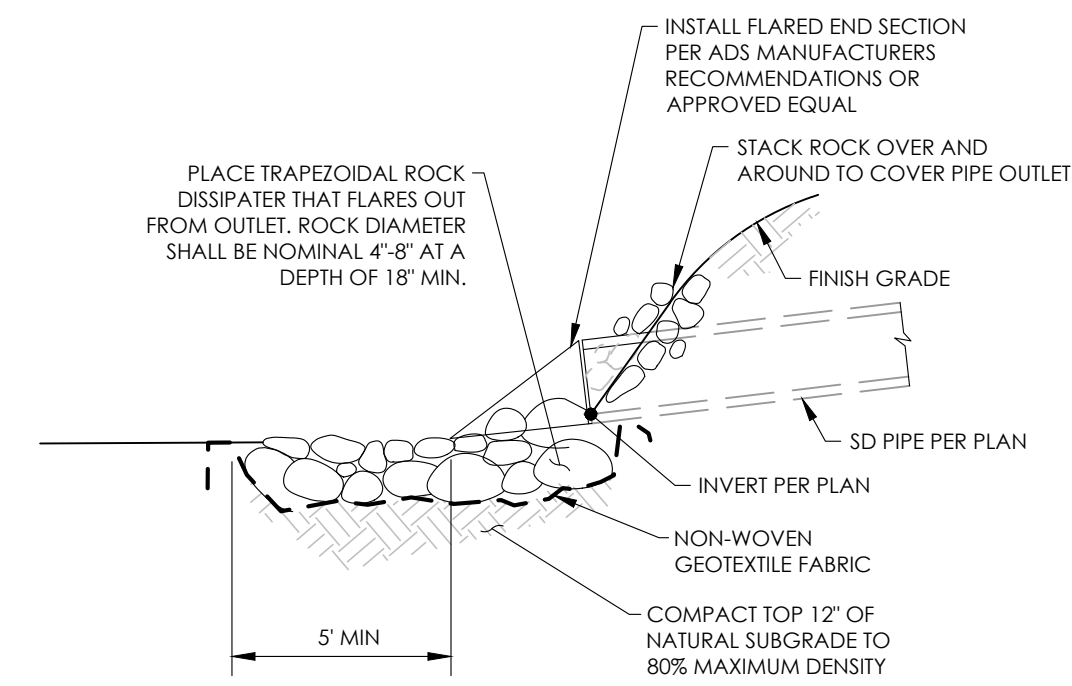
SECTION D-D

NTS



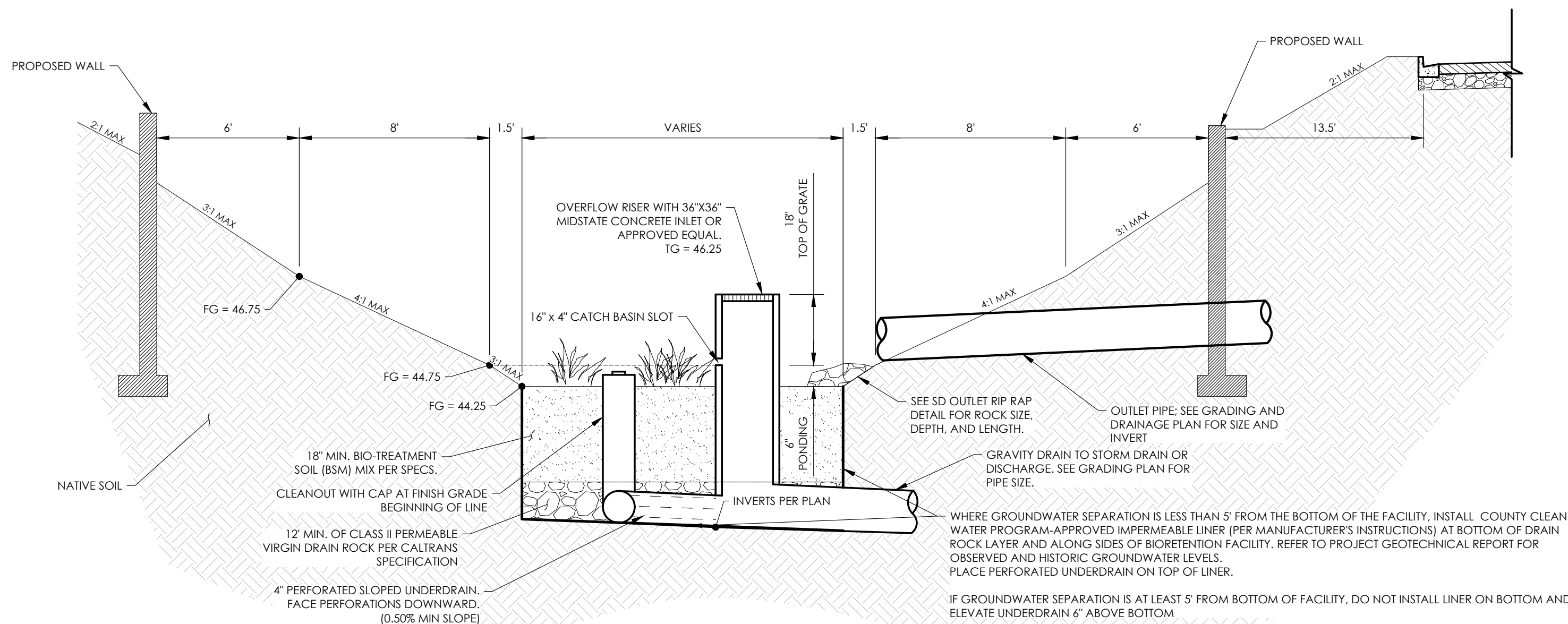
SECTION E-E

NTS



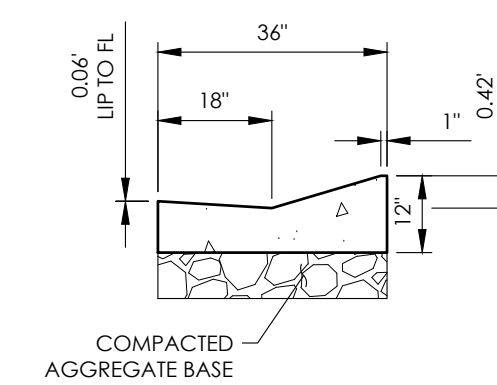
SD DRAIN OUTLET RIP RAP DETAIL (18" - 24" PIPE)

NTS



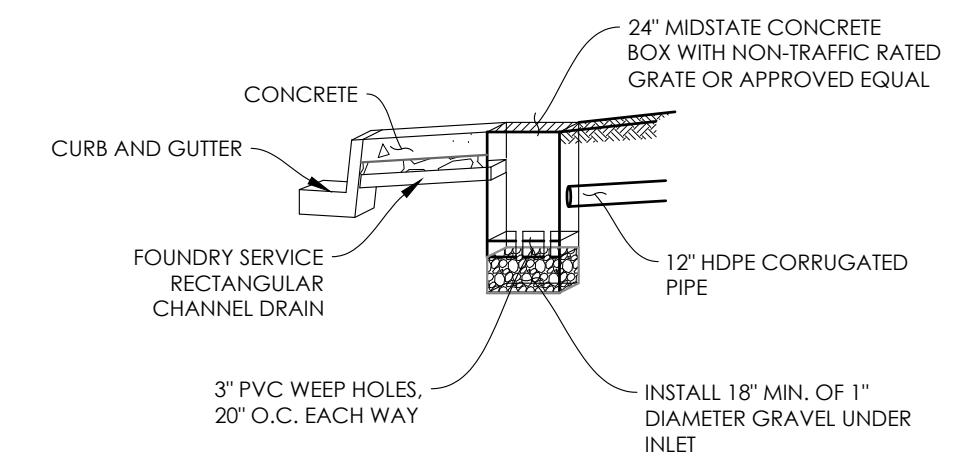
SECTION (F-F) - TYPICAL BASIN DETAIL

NTS



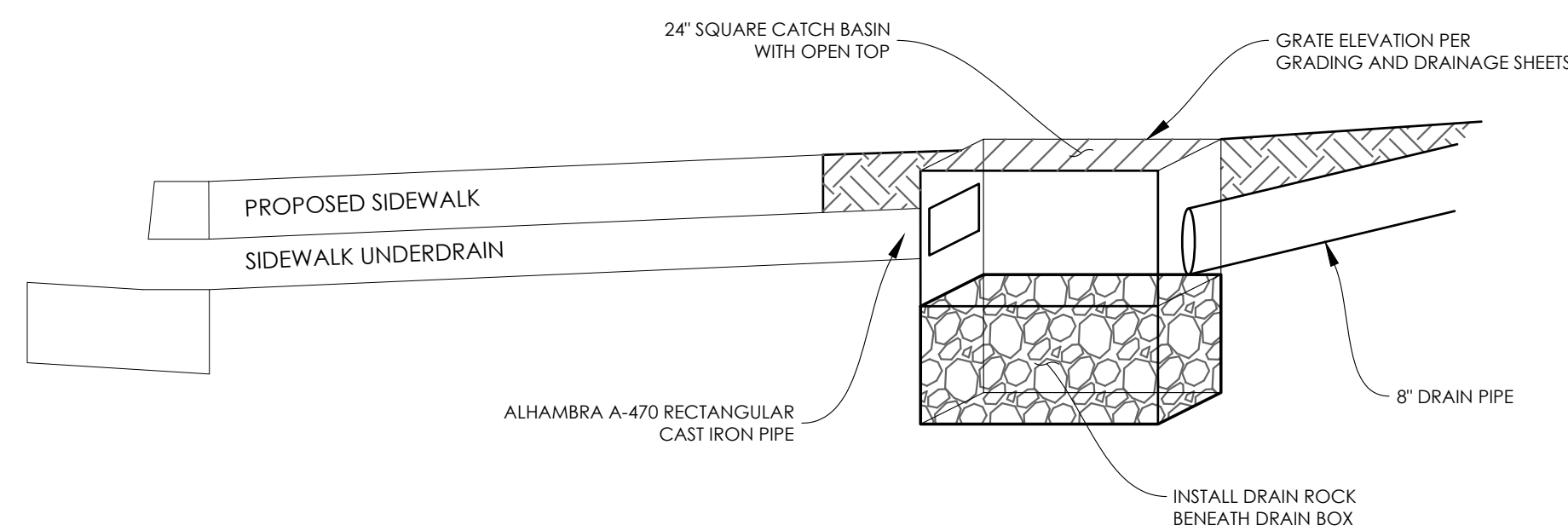
ROLLED CURB DETAIL

NTS



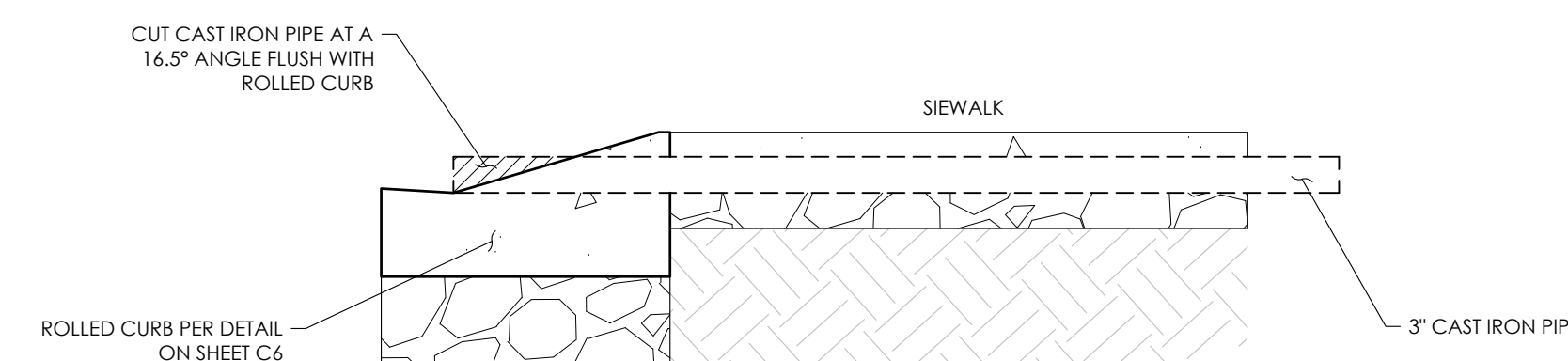
OUTFALL DETAIL

NTS



OUTFALL WITH SIDEWALK UNDERDRAIN DETAIL

NTS



SIDEWALK UNDERDRAIN DETAIL

NTS

LEGEND

W

SS

JT

SD

SS

W

PROPOSED PUBLIC 8" PVC WATER

DOMESTIC WATER METER

PROPOSED PUBLIC SEWER MAIN LINE, SIZE PER PLAN. S = 0.005 FT/FT MIN.

PROPOSED 4" SEWER SERVICE LATERAL, S = 0.02 FT/FT MIN.

APPROXIMATE LOCATION OF JOINT TRENCH

PROPOSED CCWD STANDARD FIRE HYDRANT AND FIRE HYDRANT ASSEMBLY

48" CCCSD STANDARD MANHOLE

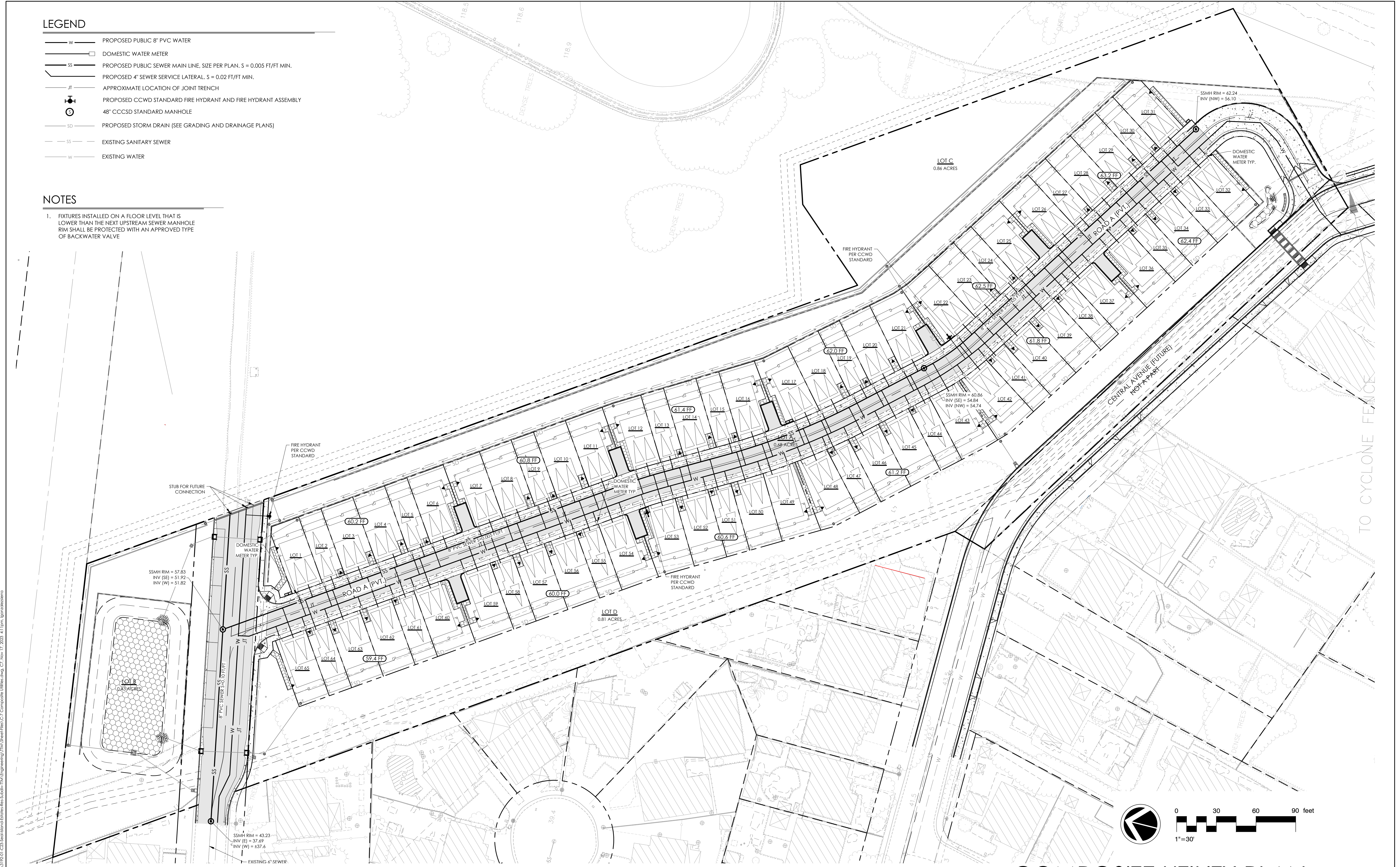
PROPOSED STORM DRAIN (SEE GRADING AND DRAINAGE PLANS)

EXISTING SANITARY SEWER

EXISTING WATER

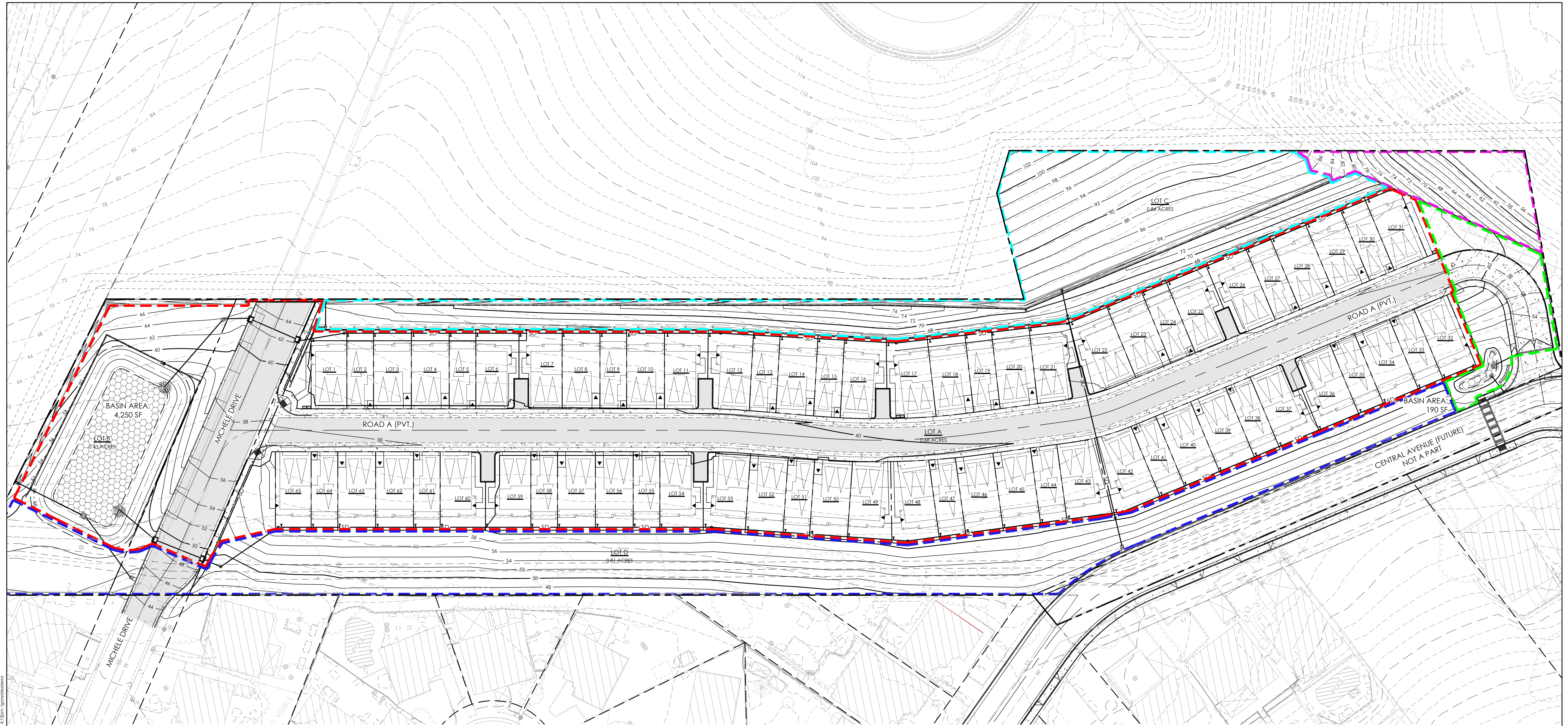
NOTES

1. FIXTURES INSTALLED ON A FLOOR LEVEL THAT IS LOWER THAN THE NEXT UPSTREAM SEWER MANHOLE RIM SHALL BE PROTECTED WITH AN APPROVED TYPE OF BACKWATER VALVE



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LEGEND

- SD PROPOSED HDPE STORM DRAIN
- DMA 1 (134,257 SF)
- DMA 2 (6,476 SF)
- DMA 3 (29,221 SF) SELF-TREATING
- DMA 4 (5,486 SF) SELF-TREATING
- DMA 5 (37,142 SF) SELF-TREATING
- STORMWATER BASIN

PROJECT STATISTICS

- (A) EXISTING CONDITION**
- IMPERVIOUS AREA: 2,583 SF
- PERVIOUS AREA: 209,999 SF
- (B) PROPOSED IMPROVEMENTS**
- TOTAL IMPERVIOUS AREA: 101,655 SF**
- TOTAL PERVIOUS AREA: 110,927 SF**

REQUIRED STORMWATER CONTROL MEASURES

- DMA 1**
 - REQUIRED BASIN AREA : 4,050 SF
 - PROPOSED BASIN AREA: 4,250 SF
- DMA 2**
 - REQUIRED BASIN AREA: 120 SF
 - PROPOSED BASIN AREA: 190 SF

